

URBAN TRAILS

NORTH RICHLAND HILLS, TEXAS

JUNE 1, 2020

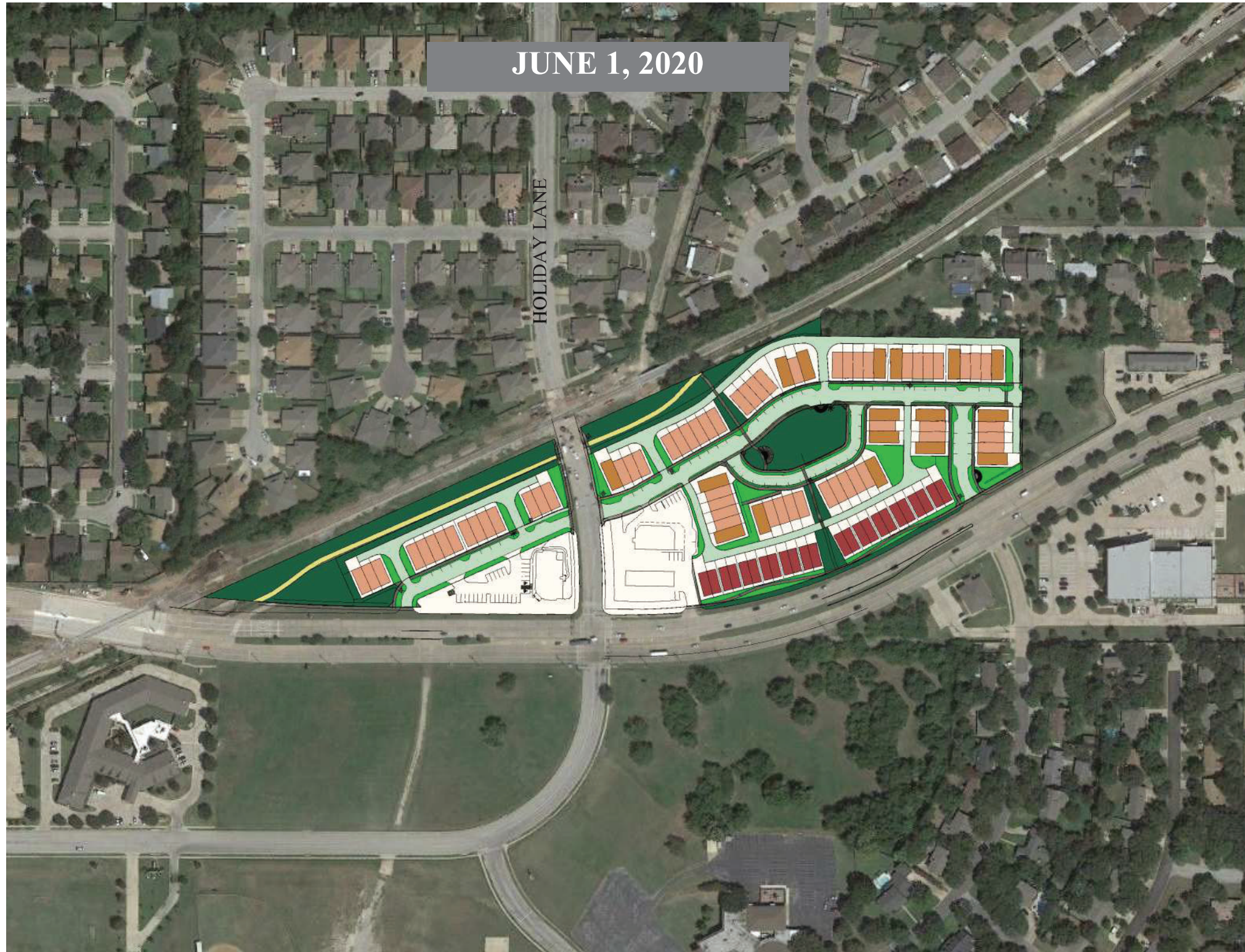
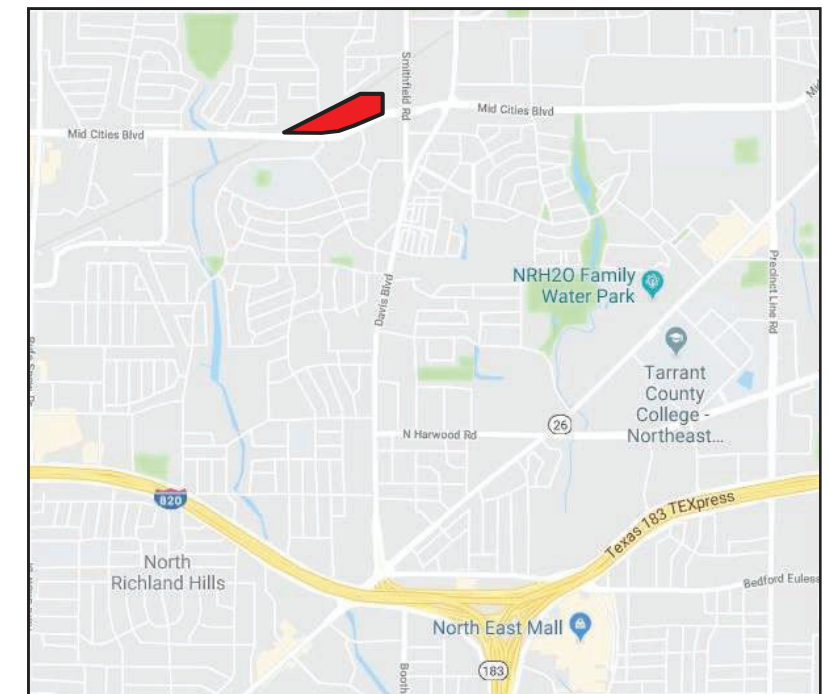


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LOCATION MAP

ADMINISTRATION

Urban Trails Master Covenant requires any development within the Declaration Area to conform with these Design Guidelines (“Guidelines”). Anyone wishing to build a new building, modify an existing building, add or change landscaping, add or change roadways or drainage, add or change signage, or make any other improvements must apply for and receive approval from the Design Review Board (“DRB”) of UT prior to proceeding with any improvements.

The Guidelines may be changed, amended or modified at any time without changing the obligation of property owners to abide by the Guidelines. Enforcement of the Guidelines and the remedies and penalties imposed by the Covenant are delineated in the recorded Master Covenants of the deed.

The Declarant or, in the absence of a Declarant, the Association of property owners, shall have the authority to appoint the member or members of the DRB. The terms and conditions of the DRB’s tenure are identified in detail in the Covenant and By-Laws.

Anyone wishing to obtain approval for proposed improvements shall submit the proposed improvements as described below: These documents shall be submitted and approved prior to the formal submittal for permitting to the City.

NEW RESIDENTIAL DWELLING UNITS:





- Prototype Plans: Typical floor plans, building elevations, and roof plans for all the typical units to be included in the building inventory shall be submitted to the DRB prior to offering any property for sale. Submittals shall include dimensioned floor plans, a typical lot plan for each lot type, and proposed categories of materials.
- Lot Plans: After approval of the Prototype Plans, and prior to submitting an application for a building permit to the City, the Applicant shall submit a lot plan for each lot. This submittal

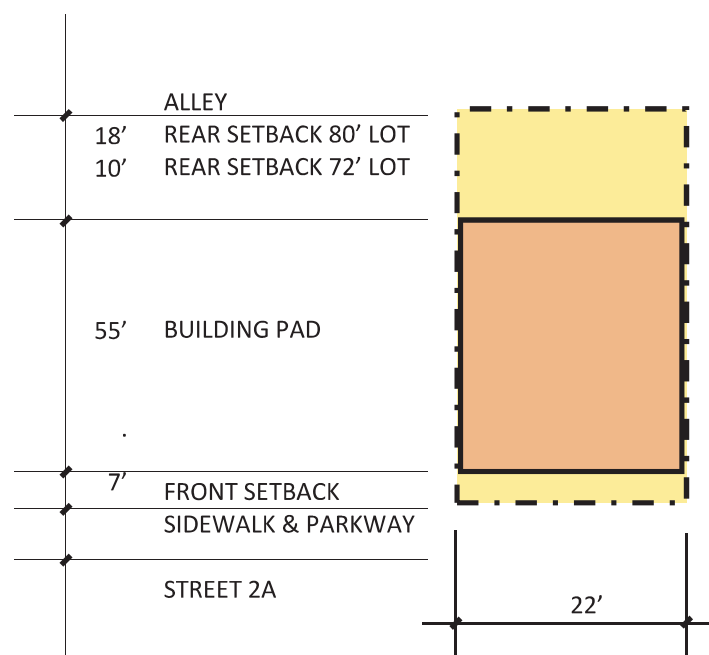
shall include the specific floor plan and elevation, the specific material selections including brick and stone selections, roofing specification, and color selections. The submittal shall also include a lot plan showing the specific house on the specific lot, proposed walkway and driveway paving, patios, landscaping, and tree planting.

- Construction: After approval by the DRB and after obtaining a Building Permit from the City, the applicant shall adhere to the Design Guidelines for all construction within the lot, and adjacent to the lot, including fencing and street trees. The Applicant shall notify the DRB of any proposed changes to the approved plans.
- Remedies: Remedies for non-compliance with the Master Covenants and/or the Design Guidelines are specified in the Master Covenants.

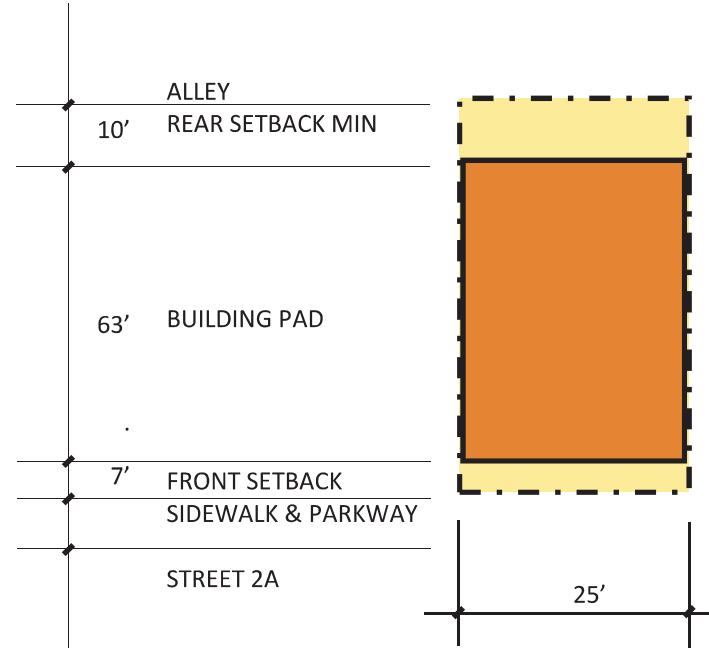
MODIFICATION OF EXISTING DWELLING UNIT

- Modification of an existing dwelling shall be subject to review and approval for all exterior modifications, with the following exceptions:
 - ◊ Replacement of roofing material with the same roofing material;
 - ◊ Re-painting with the same colors;
 - ◊ Emergency repairs to prevent further damage;
 - ◊ Seasonal or temporary decorations;
 - ◊ Replacement of dead or dying landscape materials.
- Proposed modifications shall be submitted to the DRB in accordance with procedures established in the By-Laws.

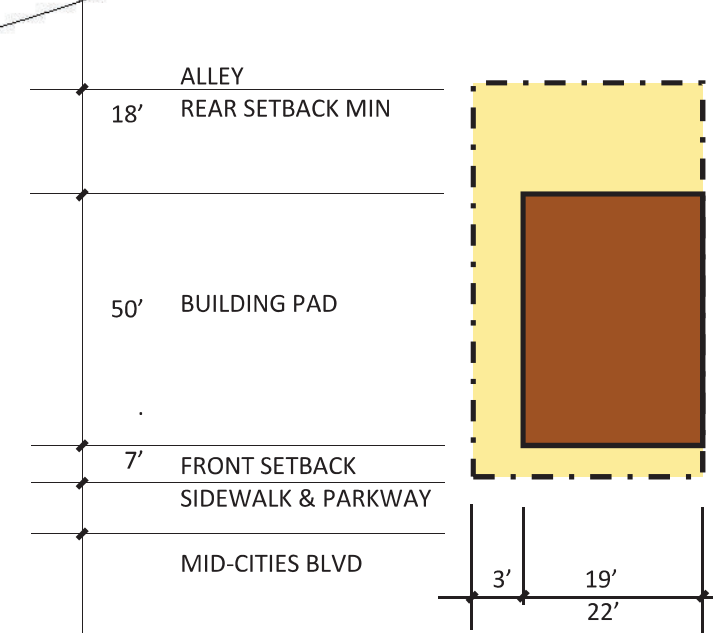
LOT SUMMARY		LOT COUNT
	22' X 72'	22 LOTS
	22 X 80'	37 LOTS
	25' X 80	17 LOTS
	22' X 75'	24 LOTS
TOTAL		100 LOTS



22' TOWNHOME LOTS

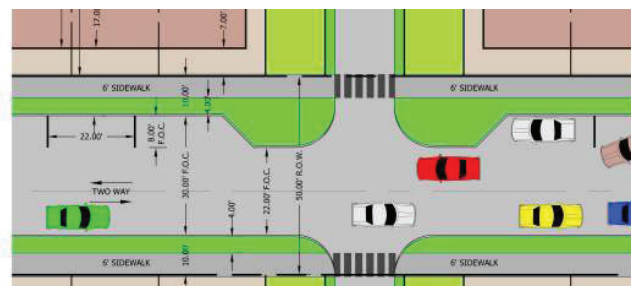
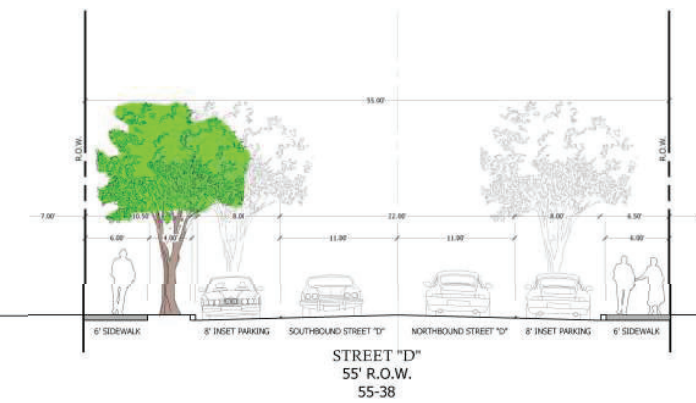
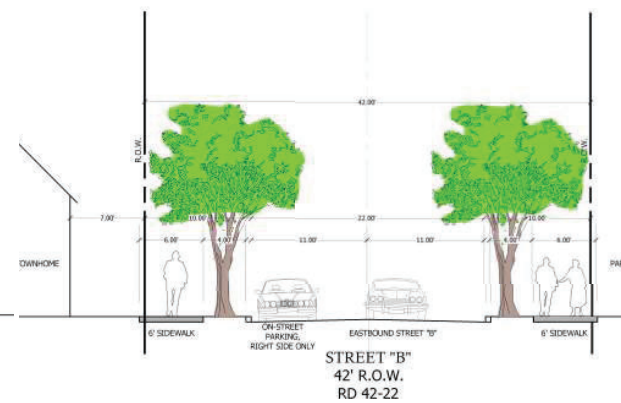
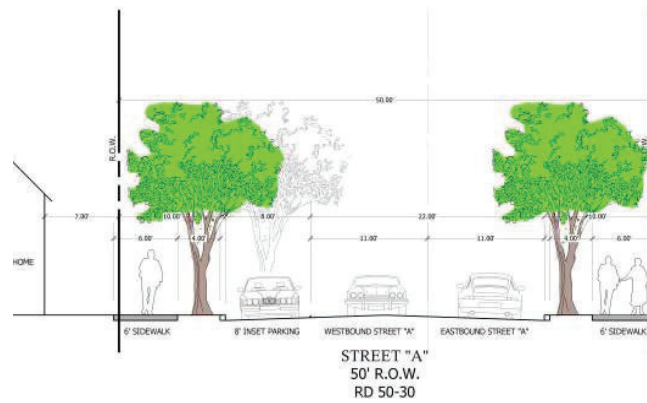
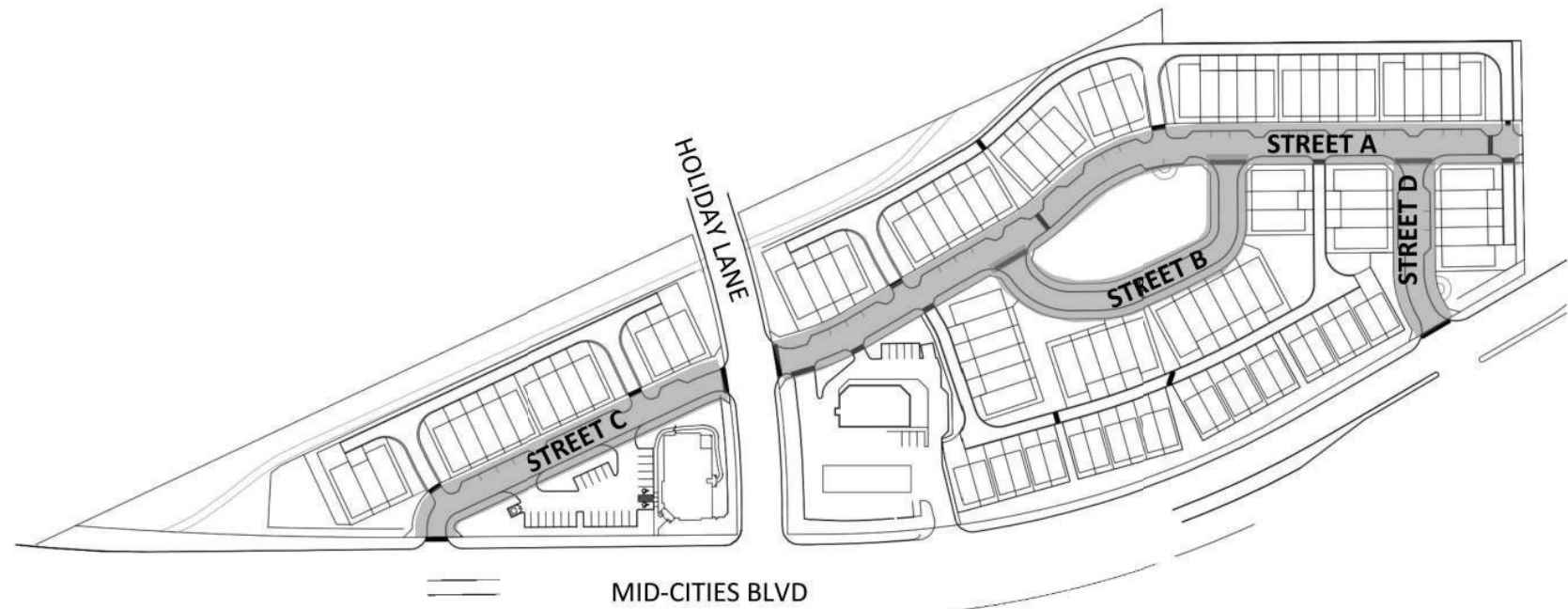


25' TOWNHOME LOTS

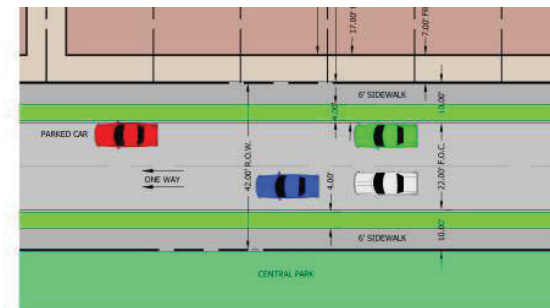


25' SINGLE FAMILY ATTACHED LOTS

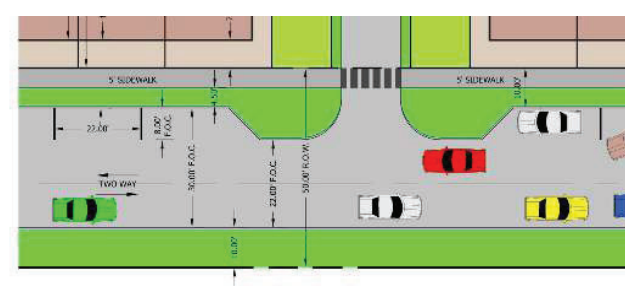
STREET TYPES		
STREET NAME	SECTION TYPE	
MID-CITIES BLVD	ARTERIAL	NO PARKING
STREET A	RD 50-30	TWO-WAY, PARKING ONE SIDE ONLY
STREET B	RD 42-22	ONE-WAY, PARKING ONE SIDE ONLY
STREET C	RD 50-30	TWO-WAY, PARKING ONE SIDE ONLY
STREET D	RD 55-38	TWO-WAY, PARKING BOTH SIDES



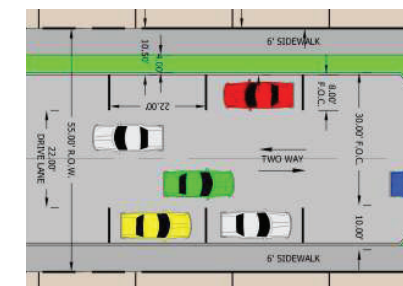
STREET A
ROADWAY TYPE RD 50-30
Two-way, Parking one side, Sidewalk both sides



STREET B
ROADWAY TYPE RD 42-22
One-way, Parking one side, sidewalk both sides







STREET C
ROADWAY TYPE RD 50-30
Two-way, Parking one side, Sidewalk one side

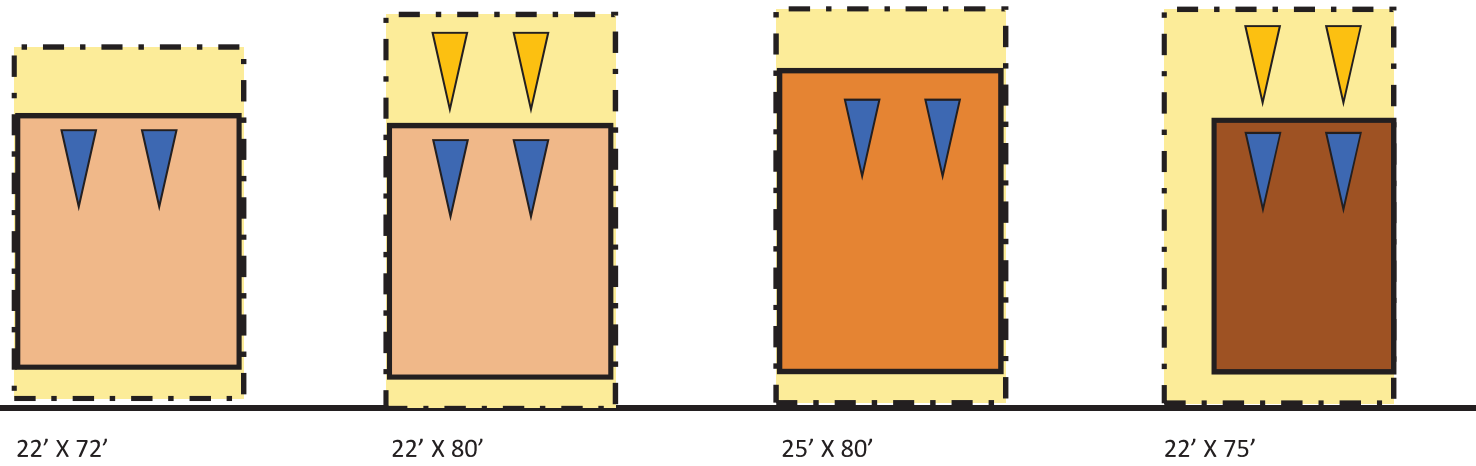




STREET D
ROADWAY TYPE RD 55-38
Two-way, Parking both sides, Sidewalk both sides

STREET STANDARDS

OFF-STREET PARKING: 3 SPACES PER LOT					
LOT TYPE		LOT COUNT	DRIVEWAY PARKING	GARAGE PARKING	TOTAL PARKING SPACES
	22' X 72'	22 LOTS	0	2 PER LOT / 44	44
	22 X 80'	37 LOTS	2/LOT x 19 LOTS = 38	2 PER LOT / 74	112
	25' X 80	17 LOTS	0	2 PER LOT / 34	34
	22' X 75'	24 LOTS	2/LOT x 24 LOTS = 48	2 PER LOT / 48	96
TOTAL		100 LOTS	86	200	286

ON-STREET PARKING		
STREET NAME	STREET TYPE	TOTAL
STREET A	RD 50-30	32
STREET B	RD 42-22	13
STREET C	RD 50-30	9
STREET D	RD 55-38	8
OFF-STREET TOTAL PARKING		62
GARAGE PARKING	2/LOT	200
DRIVEWAY PARKING		86
TOTAL PARKING		348
TOTALS		348



-  PARKING SPACE IN DRIVEWAY (REAR HEAD-IN PARKING ON A MINIMUM OF 19 INTERIOR TOWNHOME LOTS)
-  PRIVATE PARKING SPACE IN GARAGE



ON-STREET PARKING SPACES ARE 8' X 22' MINIMUM



CEDAR ELM



CHINKAPIN OAK



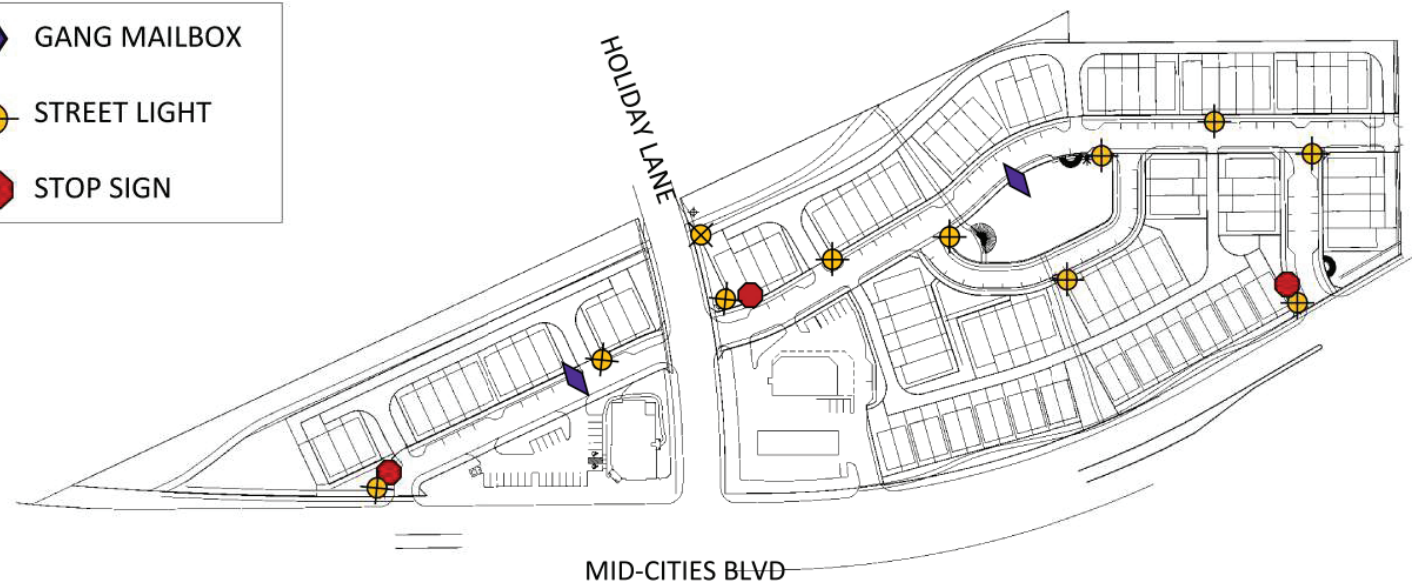
LACEBARK ELM

STREET TREES: Street trees are located approximately 30' on center along the street frontage with additional trees in the park and open space areas. The species selection and planting standards are indicated in the landscaping requirements within the Design Guidelines.

STREET TREE SPECIES SELECTION		
STREET TBD	CHINKAPIN OAK	
STREET TBD	LACEBARK ELM	
STREET TBD	CEDAR ELM	

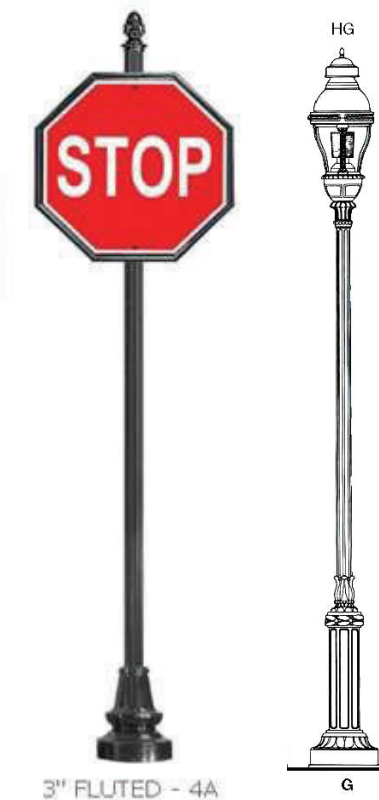
LEGEND

-  GANG MAILBOX
-  STREET LIGHT
-  STOP SIGN

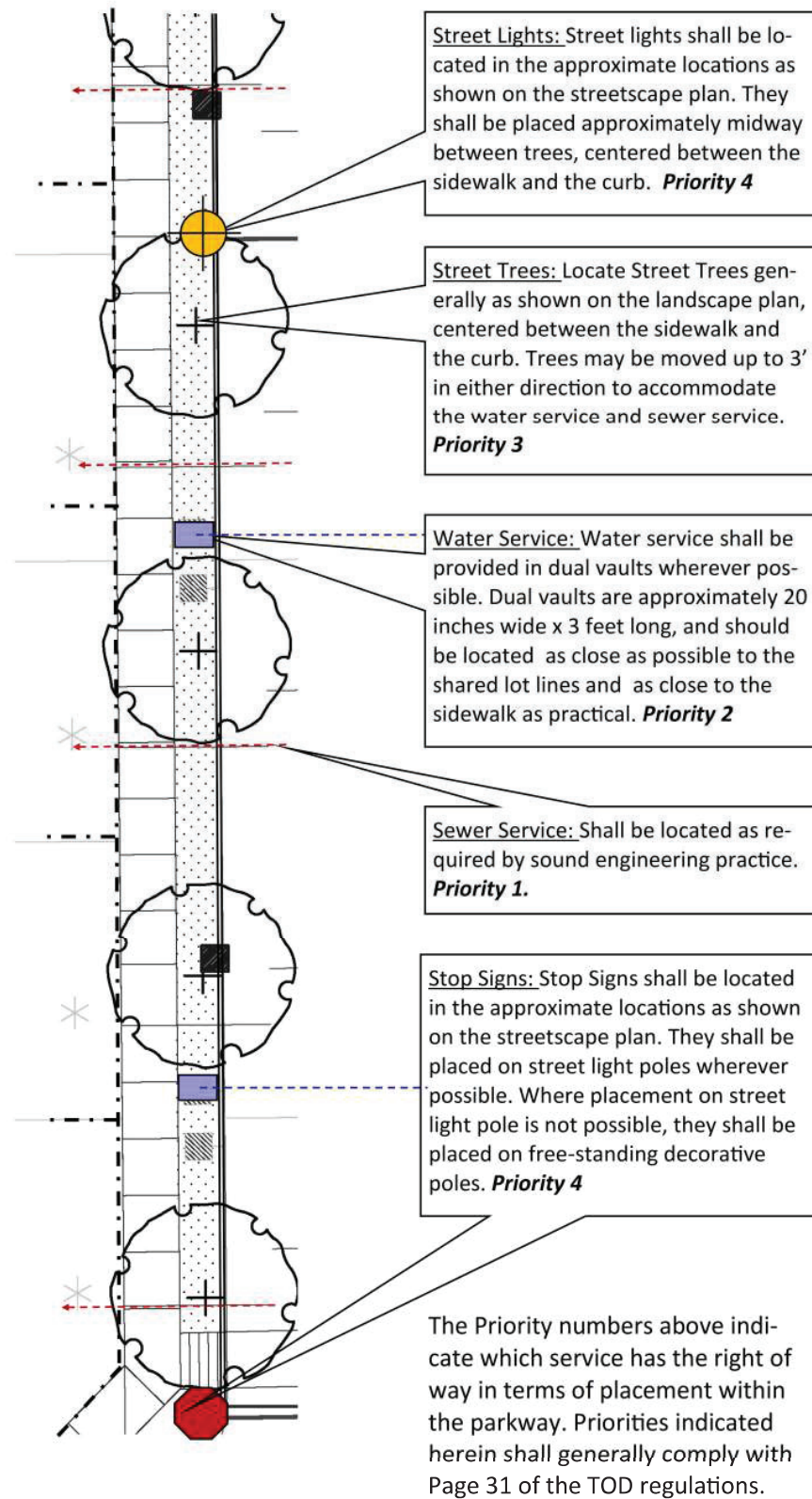


LOCATIONS OF STREET LIGHTS, STOP SIGNS AND MAIL BOXES ARE APPROXIMATE AND CONCEPTUAL.

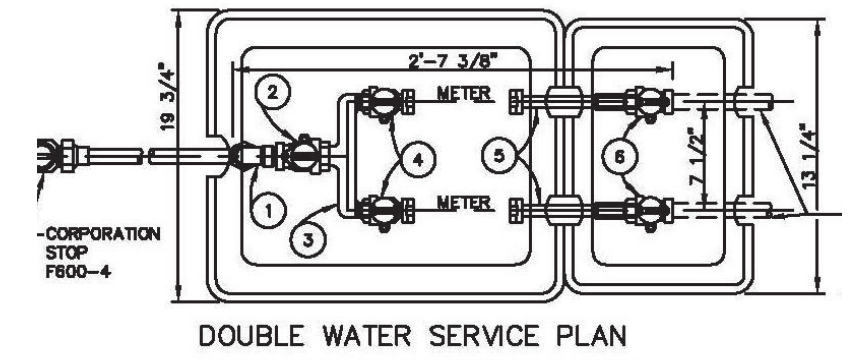
- STREET LIGHTS:** Street lights shall be provided by Oncor and shall be comprised of the following components: Poles shall be the Central Park Style poles not to exceed 12' in height, Luminares shall be the "Texan" styles, Model "H" with the full cut-off luminaire. Spacing of poles shall be placed at intersection and between intersections shall not exceed 300' as measured along the street centerline.
- MAILBOXES:** Mailboxes shall be gang boxes located in conformance with USPS requirements.
- STOP SIGN:** Stop signs are located, subject to review by the City's Public Safety staff, at appropriate intersections and shall be mounted on street light poles where possible. Where street light mounting is impractical, they shall be mounted on 3" or 4" inch fluted pole, model 4A, with cast aluminum finial, base and trims as manufactured by Brandon Industries
- TRAFFIC SIGNS:** Traffic signs shall be mounted on the street light poles where practical. Where mounting on a street light is impractical, mount sign on a 3 inch fluted pole, model 4A, with cast aluminum finial, base and trim as manufactured by Brandon Industries
- STREET SIGNS:** Street Signs shall be mounted on street light poles or on 3" or 4" inch fluted pole, model 4A, with cast aluminum finial, base and trims as manufactured by Brandon Industries
- OTHER:** See Landscape plans, submitted under separate cover, for benches, trash receptacles, bike racks, and drinking fountains.



STREETSCAPE STANDARDS



S1 TYPICAL STREETSCAPE COORDINATION



S3 DETAIL OF WATER SERVICE

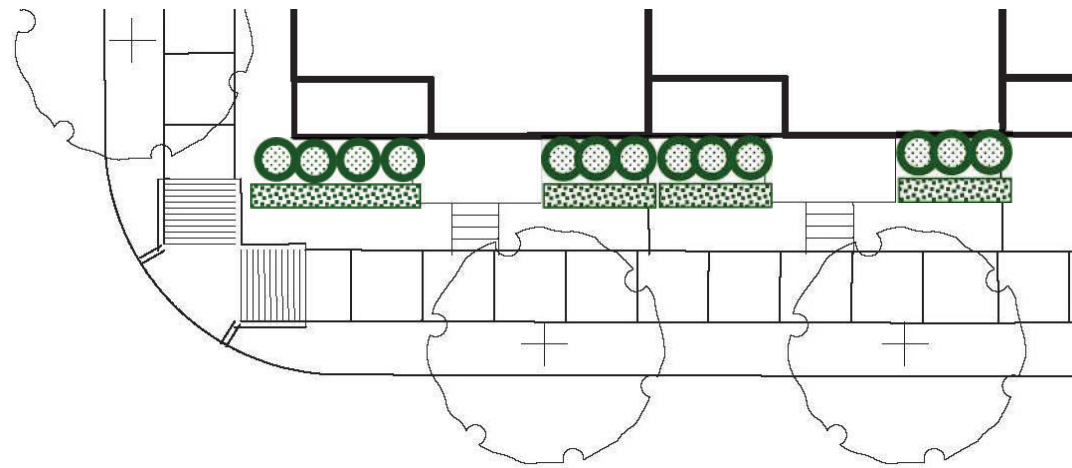
STREETSCAPE NOTES:

1. **Building Set-back Line:** The front wall enclosing conditioned space must be a minimum of 5' from the front property line,
2. **Porch or Stoop:** The Stoop or Porch must be approximately 18" above the highest curb elevation of the lot's frontage, depending on final grade.
3. **Front Yard:** The front yard may contain the lead walk and the steps up to the porch or stoop. See Landscape requirements.
4. **Sidewalk:** The public sidewalk is located within the public right-of-way, or pedestrian access easements as indicated in the street section exhibits.
5. **Parkway:** The parkway separating the sidewalk from the street shall be a minimum of 4'-0" in width.
6. **Street:** The street includes on-street parking.

S2 TYPICAL FRONT YARD STREETSCAPE

MAINTENANCE NOTES

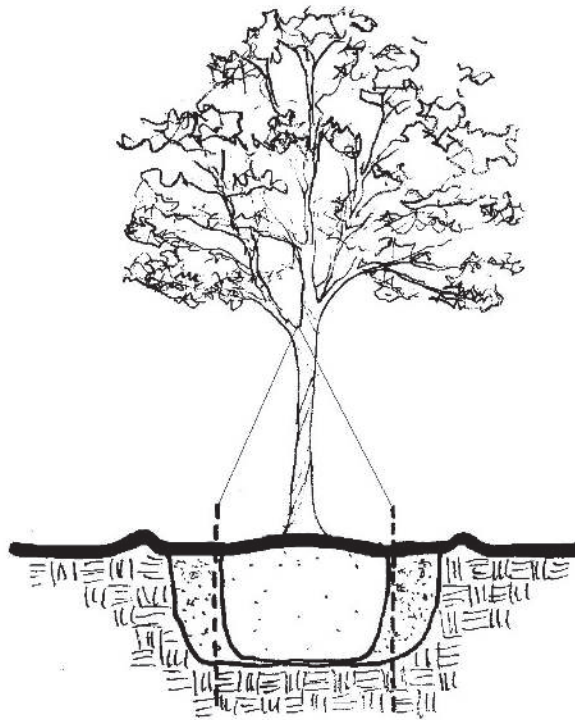
1. **Public Pedestrian ROW Frontage:** This area contains the public sidewalk and the parkway within the ROW which includes street trees, street lights, and traffic signage. See Landscape requirements for tree species identification. The sidewalk shall be located within the public ROW such that the lot property line coincides the inside edge of the sidewalk.
2. **Street Trees:** The private HOA shall maintain and, if necessary, replace street trees.
3. **Parkway:** The private HOA shall maintain landscaping and, when necessary, replace landscape materials.
4. **Sidewalk:** the private homeowner or HOA shall keep sidewalks clean and free of ice and snow. If repair or replacement is required, If repair or replacement is required, the homeowner or HOA shall work within the guidelines of the City's Sidewalk Replacement Program or other sidewalk replacement policies.
5. **Building Set-back:** The front wall enclosing conditioned space must be a minimum of 5' from the front property line,
6. **Front Yard Maintenance Easement:** The HOA shall maintain landscaping and automatic sprinkler systems between the public ROW and the front building line.
7. **Side Yard Maintenance Easement:** The HOA shall maintain landscaping and automatic sprinkler systems between the public ROW and the side building line or the privacy fence, whichever is closer to the sidewalk.



TYPICAL LANDSCAPE PACKAGE TOWNHOME; SIDE YARD FRONTAGE;		TYPICAL LANDSCAPE PACKAGE TOWNHOME; FRONT YARD:	
STREET TREE	PER PLAN	STREET TREE	PER PLAN
ORNAMENTAL SHRUBS, OR	1 OPTIONAL	ORNAMENTAL	0
ORNAMENTAL GRASSES	5	SHRUBS	6
GROUND COVER	8	GROUND COVER	8
TURF ON REMAINDER		TURF ON REMAINDER	

L1 TOWNHOME FRONTYARD LANDSCAPE

1. Provide tree stakes with bark protection at trunk and branches
2. Trees shall have single straight trunks
3. Place top of rootball 2" above finished grade
4. Provide 3" layer of mulch within watering ring
5. Remove shipping ropes, wires, and burlap from trunk and top of rootball
6. Provide a 3" minimum high earthen water retention ring
7. Excavate for rootball leaving approximately 1 foot for backfill
8. Remove frame container from rootball
9. Set bottom of rootball on undisturbed grade

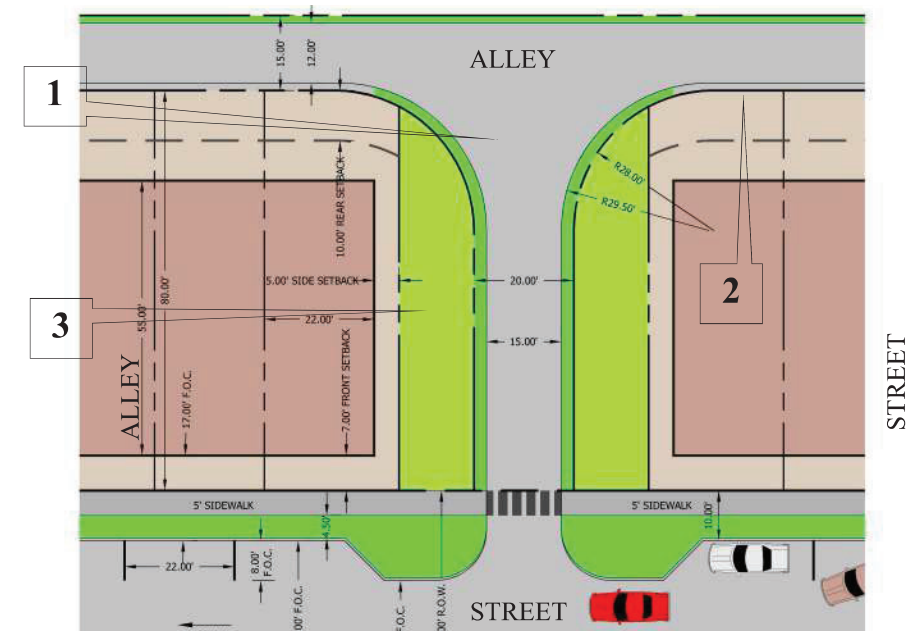


L2 TREE REQUIREMENTS

ALLEY AREAS

NOTES FOR ALLEYS:



1. Alley: Alley ROW (15') and alley paving (12').
2. Setback to building: Building setback shall maintain a minimum of 10' from the rear property line.
3. Alley Side Yard: Building setback shall maintain a minimum of 5' from the side property line to the alley ROW, or the lot will be separated from the alley by an open space lot.



L3 TYPICAL TOWNHOME LOT & ALLEY



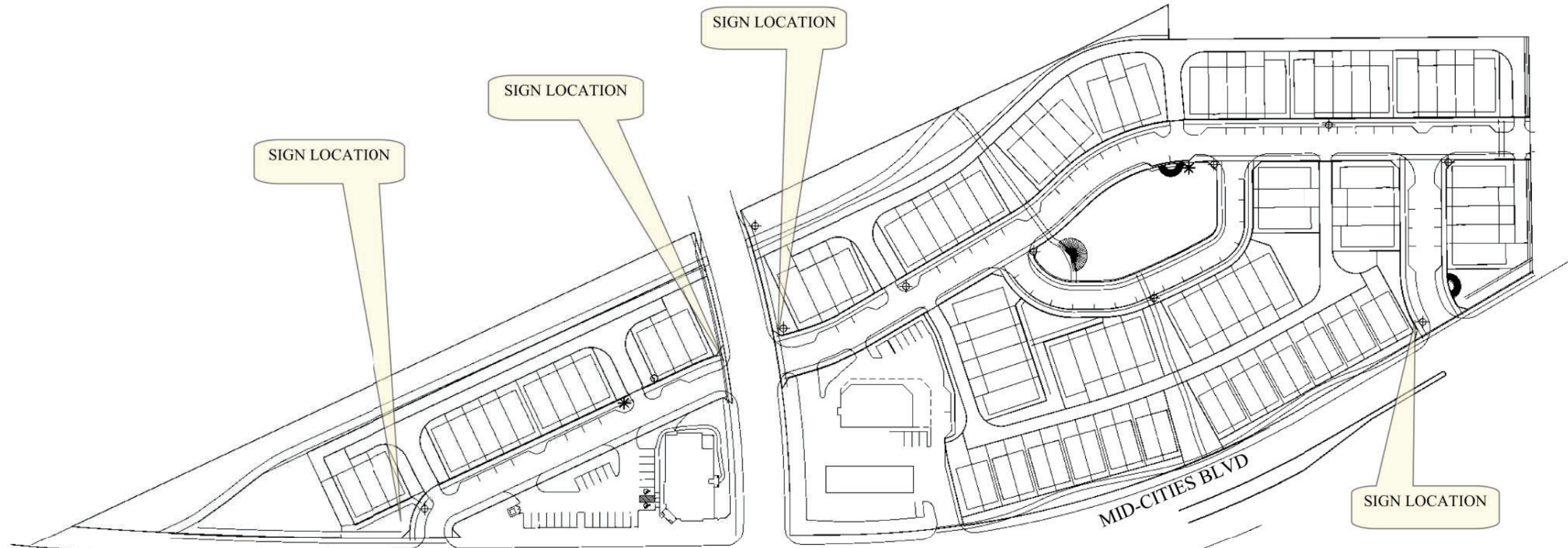
01 OPEN SPACE PLAN

OPEN SPACE TABULATION				
	GROSS AREA	REQUIRED OPEN SPACE = 5%	PROVIDED OPEN SPACE.	PERCENTAGE OPEN SPACE
OPEN SPACE	11.89 ACRES	0.59 ACRES	3.19 ACRES	29.2%
LANDSCAPED OPEN SPACE 			0.79 ACRES	7.23 %
PROGRAMMABLE OPEN SPACE 			2.40 ACRES	21.98 %

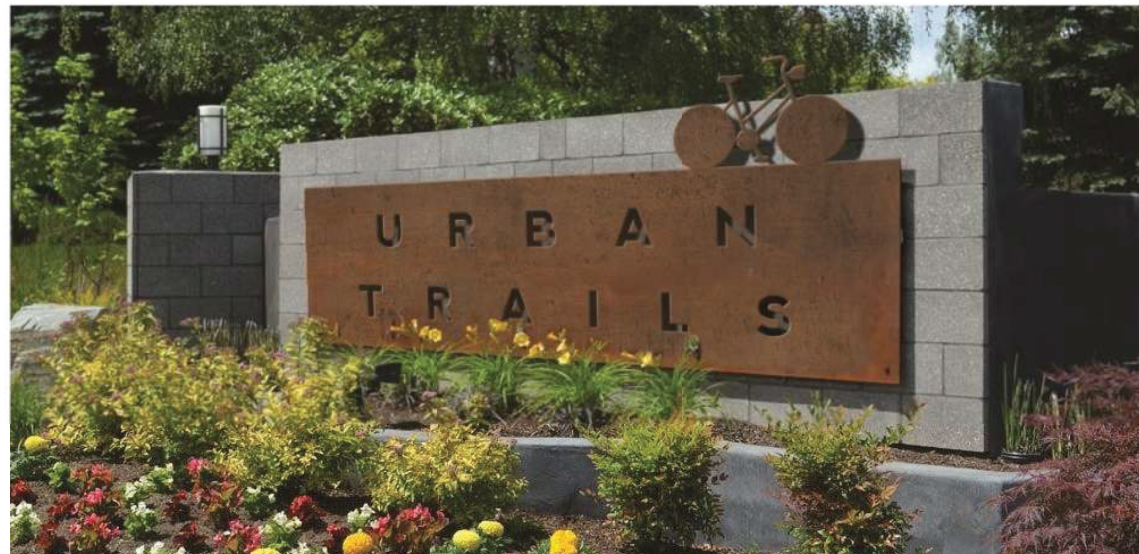
Examples of park furnishings and street lights. The selection is conceptual and represents the design intent. Final specifications shall match or be equal to the images shown.



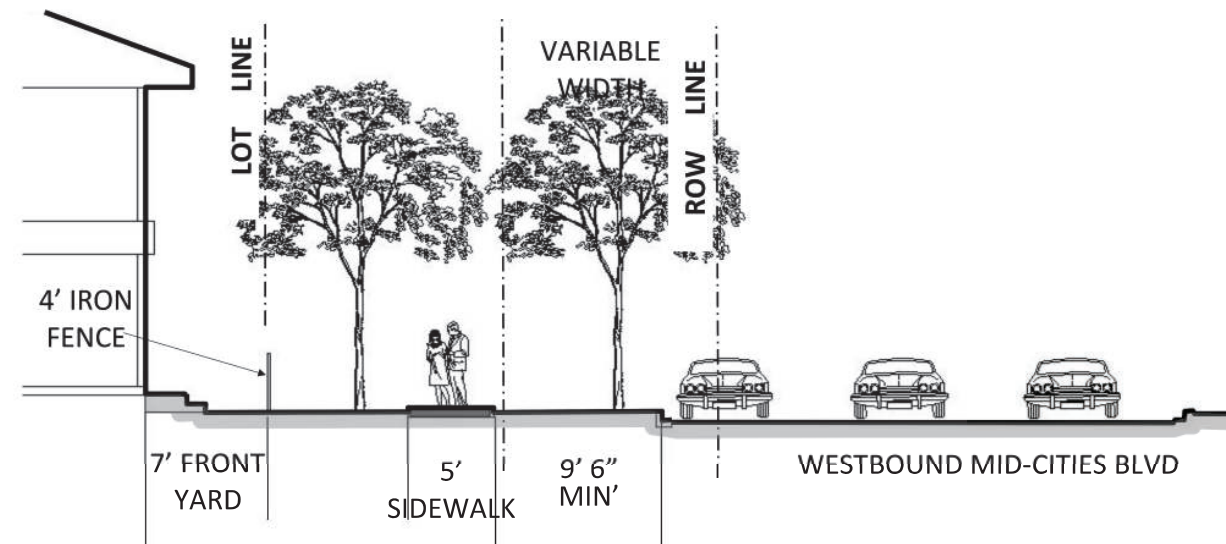
OPEN SPACE STANDARDS



F1 ENTRY FEATURE MAP



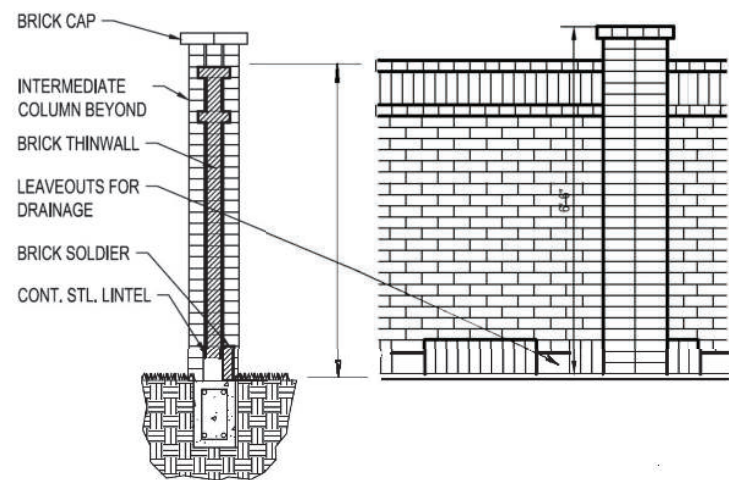
F2 ENTRY FEATURE DESIGN



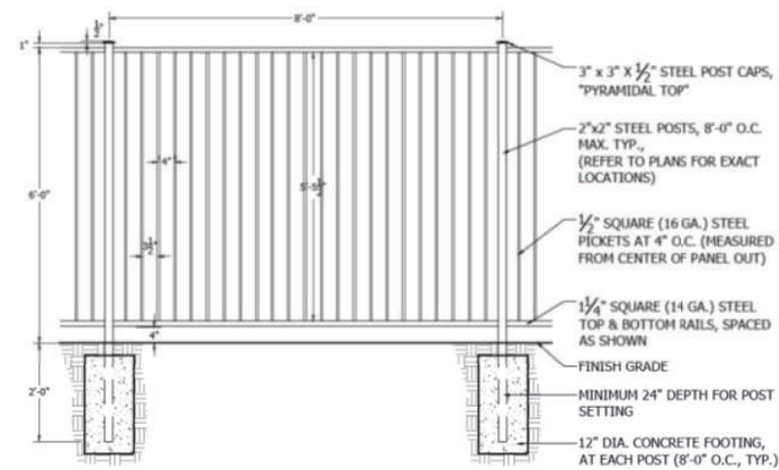
F3 STREET SECTION MID-CITIES



F4 FENCING MAP



F5 MASONRY FENCE 6' IN HEIGHT



F6 IRON FENCE 4' AND 5' IN HEIGHT



DUPLEX BUILDING FEATURES

- COACH LIGHTS AT FRONT DOOR.
- 8' ENTRY DOOR.
- CEDAR ATTIC VENTS.
- BRICK CORSE AT ROOF RAKE.
- BRICK WAINSCOT.
- 9' FIRST FLOOR PLATE, 8' SECOND FLOOR PLATE
- PIN LETTERING STREET ADDRESS.
- BRICK SURROUND AT WINDOW.
- COVERED PORCH.
- 12:12 ROOF PITCH ON FRONT ELEVATION (SIDE TO SIDE).
- MIN. 35% FIRST FLOOR WINDOW AND DOOR OPENINGS AT FRONT ELEVATION, 20% OF FIRST FLOOR AT SIDE STREET ELEVATION.
- CEMENT FIBER VERTICAL BOARD AND BATTEN SIDING.

TOWNHOME BUILDING FEATURES

ALL TOWNHOMES WILL BE COMPLEMENTARY TO DUPLEX BUILDINGS AND MEET TOD CODE BUILDING MATERIALS REGULATIONS. TOWNHOMES WILL HAVE SAME 9' AND 8' BUILDING PLATES AND WINDOW AND DOOR FIRST FLOOR OPENING REQUIREMENTS; 10:12 FRONT ELEVATION SIDE TO SIDE ROOF PITCH; NO REQUIRED PIN LETTERING ADDRESS AND NO REQUIRED COACH LIGHTS. CEMENT FIBER VERTICAL BOARD AND BATTEN SIDING AS SHOWN ON DUPLEX ELEVATION ALLOWED.

URBAN TRAILS TOWNHOMES BUILDING MATERIALS

At least 75% of each façade (excluding doors and windows) visible from any public street shall be finished in one of the following materials. No more than any combination of three of the following different materials shall be used on any single façade.

- Masonry (brick, stone, man-made stone. Masonry materials shall be installed in a craftsman like manner that are a minimum of one inch thick and imbedded in a cementitious reinforced substrate.
- Cementitious vertical board and batten siding with a paintable finish and a 50-year warranty shall be restricted to upper floors and no more than 50% of the block face on the first floor of any building block face.

The following shall be only allowed up to 25% of each façade as an accent material:

- Decorative wood (natural resistant to decay), metal, synthetic materials or reinforced Exterior Insulating Finish System (EIFS) or similar material over a cementitious base, rock, glass block and tile.

COMPLIANCE WITH MINIMUM GLAZING REQUIREMENTS: CALCULATION:

DENOMINATOR: THE WIDTH OF THE HOUSE FRONTING A STREET TIMES THE DISTANCE BETWEEN THE FINISHED FIRST FLOOR AND THE TOP PLATE OF THE FIRST FLOOR FRAMING

NUMERATOR: SUM OF ALL FIRST FLOOR OPENINGS, INCLUDING PORCH OPENINGS, WINDOWS, ENTRYWAYS, AND THE DECORATIVE FEATURES SURROUNDING THOSE OPENINGS SUCH AS LINTELS, AND SILLS.

RATIO: THE RATIO OF 1ST FLOOR OPENINGS TO FIRST FLOOR FAÇADE AREA SHALL NOT BE LESS THAN 35% ON FRONT ELEVATIONS. ON SIDE ELEVATIONS FRONTING A SIDE STREET ON A CORNER LOT THE MINIMUM RATIO SHALL BE 20%.



CENTRAL PARK LANDSCAPE



MID-CITIES BOULEVARD LANDSCAPE